

2040 Comp Plan Update: Mobility & Transportation

Goal 1: Create connected, livable neighborhoods that have multi-modal transportation options for all residents. *[Sustainable Goal inclusive of all policies]*

POLICIES (All policies sustainable)

#1: Move toward implementation of a complete streets network that is safe, convenient and attractive for everyone regardless of age, ability or mode of transportation.

***Side Bar: What are Complete Streets?** Complete Streets are streets that provide safe, convenient, and comfortable routes for all users, regardless of age, ability, or mode of transportation. A network of complete streets makes the transportation network safer and more efficient for everyone, including pedestrians, bicyclists, motorists and transit riders of all ages and abilities. A “Complete Street” right-sizes infrastructure based on functional classification and traffic volumes and may include the following features: sidewalks, multiuse paths, bicycle lanes, automobile lanes, street trees, public transportation facilities, and traffic calming devices.*

#2: Continue to promote walkable neighborhoods that provide transportation choices and convenient access in existing and future sections of the City.

#3: Discourage cul-de-sac streets in favor of connected streets that provide transportation flexibility and increased safety in case of an obstructed street. Additionally consider mid-block sidewalks/multi-use trail connections for pedestrians for interior streets/cul-de-sacs.

#4: Require active transportation connections to every development project, and encourage additional amenities (e.g., bike racks, bike repair stands, hydration stations, etc.) in developments, public parks and other public facilities where feasible/appropriate).

***Side Bar: What is active transportation and how do we plan for it?** Active transportation is any self-propelled, human-powered mode of transportation, such as walking and biking.*

ACTIONS

#1: Continue to implement a program to identify and repair broken and substandard sidewalks throughout the City.

#2: Track and share progress annually of achievements of the City’s Complete Streets Policy. Provide annual update to Plan Commission on progress; tie update to City’s yearly Capital Improvement Program budget/programming schedule.

Goal 2: Link City residents to jobs, services and other regional amenities through a multi-modal transportation system.

POLICIES

#5: Continue to work cooperatively with the Wisconsin Department of Transportation, East Central Wisconsin Regional Planning Commission, Fond du Lac County and adjoining units of government in planning for improvements, especially to Interstate 41/USH 41 and USH 151.

#6: Maintain good access from regional highway system to the Downtown and other business/employment districts within the city, especially maintaining access from Interstate 41/USH 41 to Main Street.

#7: Ensure transportation systems meet the needs of local businesses and industries. Evaluate Fond du Lac Area Transit system as appropriate for performance measures/metrics and policy changes to enhance rider experience.

#8: Maintain the safety and efficiency of existing transportation corridors while also seeking safe and efficient travel in the City for residents.

#9: Connect gaps in multi-modal transportation networks that connect Fond du Lac to neighboring communities, for both off- and on-street facilities such as future connections with Old Plank Road Trail expansion (in coordination with WIS 23 Expansion and WisDOT) and possible expansion of Eisenbahn State Trail connection from Eden to Fond du Lac.

#10: Review infrastructure improvements based on consistency with the Bike & Pedestrian Plan.

ACTIONS

#3: Continue to promote the need for an interchange at the south end of the Fox Ridge Business Park to WisDOT through regional transportation planning efforts.

#4: Annually plan for new connections that increase resident access to regional connections in the bike and trail network. Work with neighboring jurisdictions, WisDOT, the DNR, and other invested partners to continually expand and integrate the network.

#5: Collaborate with responsible jurisdictions to ensure roadway improvements (including County and State highways) have multi-modal aspects integrated into planning and development, or appropriate alternatives developed.

#6: Continue to collaborate with the Fond du Lac MPO in updating the Long Range Transportation Plan and Transit Development Plan.

#7: Evaluate the City's transit system annually, making sure the system is efficient and cost effective in meeting the demands of all City residents.

Goal 3: Improve public health and safety through an integrated public transportation network.

POLICIES

#11: Seek to minimize conflicts between motorized and non-motorized traffic by improving street crossings, using off-street paths, and implementing protected lanes where appropriate. Prioritize investment where they will serve the daily needs and interests of residents by improving access to daily destinations such as jobs, schools, grocers, medical services, etc.

#12: Manage access to existing and future major arterials (per City Official Map) to maintain safety and operational efficiency.

#13: Design, build and operate the City's transportation system to support safe and timely response to emergencies.

#14: Design and retrofit local streets with traffic calming features, where necessary and appropriate.

#15: Collaborate with Fond du Lac School District on safe transportation for students, including walking, biking and bussing, both in neighborhoods and near each school site.

#16: Provide convenient, affordable transportation options that enable people of all ages and abilities to access jobs, services and other destinations to meet their daily needs. This should include options for people without access to a personal vehicle.

#17: Seek compliance with the Americans with Disabilities (ADA) Act whenever an existing facility is reconstructed.

ACTIONS

#8: Update the City's Capital Improvement Plan on an annual basis to plan for short-term transportation improvements, considering actions/recommendations outlined in the City's Bike & Pedestrian Plan.

#9: Update the Official Map as need/appropriate to show long-term transportation improvements and development into new growth areas.

#10: Partner with the Fond du Lac School District to provide bicycle education to students annually.

#11: Partner with bike advocacy groups to support and expand education/encouragement programs throughout the City.

Goal 4: Reduce the environmental impact of the transportation system.

(Sustainable Goal – inclusive of all policies below it)

POLICIES (All sustainable)

#18: Consider use of “green street” principles in new and reconstructed streets to minimize stormwater runoff as appropriate and where specific site conditions allow.

***Side Bar: What is a Green Street?** A green street is a stormwater management approach that incorporates vegetation (perennials, shrubs, trees), soil, and engineered systems (e.g., permeable pavements) to slow, filter, and cleanse stormwater runoff from impervious surfaces (e.g., streets, sidewalks). Green streets are designed to capture rainwater at its source, where rain falls. Whereas, a traditional street is designed to direct stormwater runoff from impervious surfaces into storm sewer systems (gutters, drains, pipes) that discharge directly into surface waters, rivers, and streams. (epa.gov)*

#19: Leverage new technologies that can efficiently manage and improve transportation networks in the City, such as intelligent transportation systems (e.g. enhanced signal coordination).

#20: Promote alternatives to single-occupancy vehicle use through strategic investments in alternative transportation, public- and employer-based commuting programs (e.g., Wisconsin’s RIDESHARE program and vanpools) and other similar programs.

ACTIONS

#12: Evaluate the use and potential impacts of new technologies in street and development projects. For instance, consider the likelihood that ride hailing services and autonomous vehicles will increase the need for pickup and drop-off space near building entrances.

#13: Review the City’s off-street parking requirements periodically (at least every two years) to assess their effectiveness in making efficient use of land for vehicle parking. When appropriate, reduce minimums and consider enacting maximums to avoid excess parking spaces

#14: Evaluate potential for, and funding of, plug-in outlets for electric vehicles in City owned lots.

MOBILITY & TRANSPORTATION

2020 SNAPSHOT

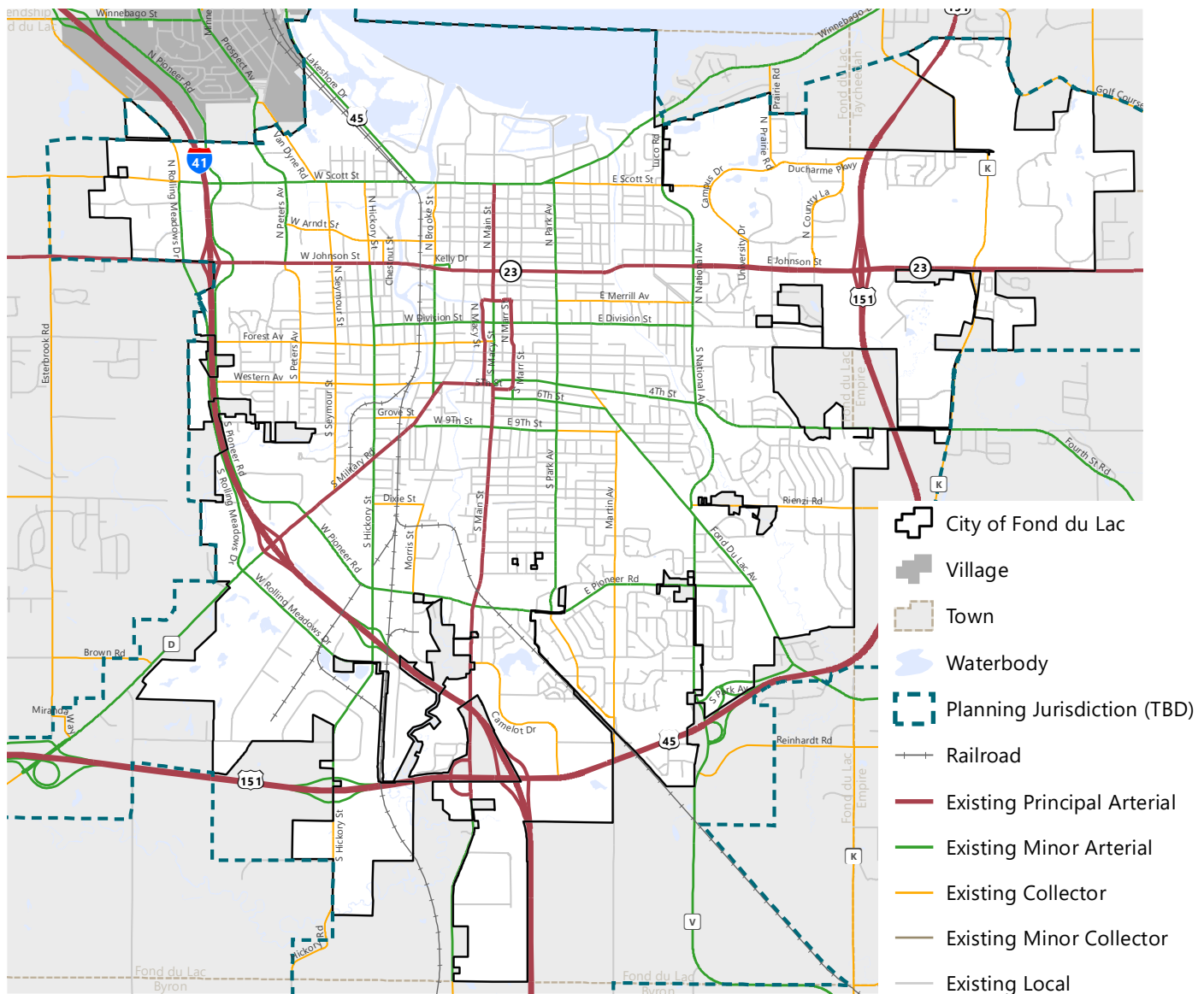
Road Network

Regional Highway System: Major highways located in Fond du Lac include US Highway 151, USH 41/Interstate 41, two state trunk highways (WIS 23 and WIS 45), and seven county trunk highways (CTH D, CTH K, CTH T, CTH V, CTH VV, CTH VVV, and CTH OOO).

Workforce Commuting Data: Based on 2017 Census Bureau data, the City of Fond du Lac has roughly 10,200 residents commuting out of the city for work (57% of employed residents) and about 17,330 people commuting into Fond du Lac for work (i.e., 69% of city jobs filled by non-Fond du Lac residents). The majority of Fond du Lac workers live in the Fond du Lac area, with many commuting from North Fond du Lac, Taycheedah, Oshkosh, and the Town of Fond du Lac.

Truck Routes: The Fond du Lac Municipal Code includes a list of the streets and highways that are designated as heavy traffic (truck) routes. State and county highways are established truck routes by the nature of their intended use and street design. However, any public street or alley may be used by heavy vehicles for the purpose of delivering or moving commodities provided they travel the shortest possible route from designated heavy traffic route to residence or place of business.

Existing Transportation Infrastructure



Bicycle & Pedestrian Network

Regional Trails: There are several existing regional routes connecting Fond du Lac to surrounding areas:

- » **Mascoutin Valley State Park Trail:** The section nearest Fond du Lac is a 32-miles, passing through the Eldorado Marsh to Rosendale.
- » **Peebles Trail:** Connects to St. Peter on a section of abandoned railway.
- » **Wild Goose State Park Trail:** A 34-mile limestone trail that runs on an abandoned railway through rural portions of the county.

Planned future trail sections include **Old Plank Road Trail** (connection to Prairie Trail), as part of WisDOT's WIS 23 Expansion Project, and a potential connection to **Eisenbahn State Trail** between Fond du Lac and Eden.

City Bike System: The City has both on- and off-street bike facilities, with the majority of the network consisting of on-road bike routes. These facilities provide strong connections both east-west and north-south and are located along some arterial and collector streets. The primary connecting path in Fond du Lac is the Fond du Lac Loop:

- » **Fond du Lac Loop:** Looping through the City through a system of 5 connected trails and roadways, the Fond du Lac loop offers nearly 16 miles of trails. This Loop also connects to major regional and local recreational trails, and is well connected to the bike route network.

Sidewalks: The City requires sidewalks in new residential and commercial development, including developed land annexed to the City. There are exceptions to the ordinance, depending on land use and future schedule of road reconstruction. The Subdivision Ordinance requires sidewalks on both sides of all new public streets. As streets are reconstructed or significantly improved within the City, sidewalks are typically installed if they are not already present, with exceptions for high-volume roads that typically are used solely for vehicle traffic. The City has the authority to grant waivers, generally in response to unusual site conditions.

In addition to new sidewalk installations, other pedestrian facility improvements such as sidewalk ramps, crosswalks, signage, etc are being upgraded when new development occurs adjacent to the facility, as part of road reconstruction, or as part of other improvement projects outlined in the City Capital Improvement Program

Transit Service

FDL Area Transit: The transit agency operates 8 transit (bus) routes throughout the City, primarily designed for typical work week travel. Seven of the routes provide at least hourly week-day service until 6:30 p.m., while Route 120 is a school route operating during school days only.

Handivan: FDL Area Transit offers an advanced reservation service that is wheelchair-lift enabled for those unable to use regular transit.

Fond du Lac Shared Ride Taxi: Fond du Lac Area Transit contracts with a private taxi service to assist residents who live more than 3/4 mile from a fixed bus route or are traveling to an area more than 3/4 mile from a fixed bus route. Areas serviced by this program are largely peripheral areas of the City, as well as North Fond du Lac. This service must be reserved in advance, although on-demand rides are accommodated when possible.

Regional Transit Service

Intercity Bus: Private intercity bus services stop in Fond du Lac, though there is no permanent bus station.

Air Service: The Fond du Lac Skyport provides recreational and charter service, located West of highway 41 in Fond du Lac.

Freight Rail: There is a major Canadian National train yard located in North Fond du Lac that provides connection throughout the region. The City has 32 train crossings, and approximately 35-40 trains per day travel the main line.

VOICES FROM THE COMMUNITY

Road & Traffic Improvements

The Community Input Mapping Tool identified many sites of road & traffic improvements - with over 130 suggestions for each.

Though many of the road & traffic improvements had to do with pedestrian safety & access, the pedestrian categories received 64 comments and the bike category 64 comments.

Common themes include:

- Need for Increased Bike Connections, including on-street facilities downtown and connecting trails
- Sidewalk additions are still needed in many areas
- General need for increased pedestrian crossings
- Need for general road repairs in several areas

95%

of all survey respondents who travel to work drive in a car alone to get there

Improved Biking

The 2019 Community Survey asked people how they would rate aspects of their section of the City - one of which was "bikeability". Of all respondents, only 50% rated the bikeability of the City as excellent or good - and some planning areas scored as low as 35%.