



Welcome to the online public involvement event for Project Design ID 4986-00-24 along Military Road in the City of Fond du Lac. This project is one of three major segments of Military Road reconstruction that the City is planning between Seymour Street and Western Avenue. The City held a Public Involvement Meeting for the entire corridor in October 2019. This current online Public Involvement Event focuses on the segment between Superior Street and Western Avenue, which construction is currently planned for 2022.

This is the 1st of two public involvement events for this segment. We are currently at approximately 20% into the design development of this portion, and the 2nd public involvement event later in the process will be able to provide additional specific details on the proposed work.

Project Team



City of Fond du Lac

Paul De Vries, PE



Wisconsin Department of Transportation

Brian Edwards, PE



MSA Professional Services, Inc.

Quirin Klink, PE



This is a list of the key personnel working on this project. You may contact Paul De Vries at the City of Fond du Lac, or Quirin Klink at MSA Professional Services, Inc. for any comments, questions or concerns. Their contact information is provided in the last slide of this presentation.

Presentation Overview

- Project Location and Background
- Proposed Improvements
- Intersection Evaluation
- Proposed Traffic Impacts
- Next Steps



The presentation will follow the outline of topics shown here.



Project Location and Background

First, we will discuss the project location and background.

Project Location and Limits



The proposed project lies fully within the City of Fond du Lac, along Military Road. The project begins just south of Superior Street, north of the railroad tracks. The project extends northeasterly approximately 0.42 mile to a point just north of the intersection with Western Avenue.

Areas of Interest

Section 4(f) Resources



The adjacent properties in this area are mostly residential, with some commercial properties. Also, there are several areas of interest. The Franklin School, a historic building, is located at the south end of the project. Franklin Park is located near the middle of the project. And a multi-use Path crosses at Brooke Street near the north end of the project. These are also called “Section 4(f)” resources. Section 4(f) properties are publicly owned parks, recreation areas, wildlife or waterfowl areas and any significant historical or archeological site.

Project Background and Goals

- Background
 - Existing pavement poor condition
 - Standard maintenance no longer sufficient
 - Nearby school, parks and recreational trails attract bicyclists and pedestrians
- Goal
 - Address roadway deficiencies while enhancing multimodal accommodations and safety



The main reason for the project is poor pavement condition, with many cracks, spalls, and potholes in the existing concrete pavement. The extent of the deterioration is such that standard maintenance is no longer sufficient. The existence of the school, park and trail are background features to consider for multi-modal transportation. Currently, there are numerous sidewalk curb ramps and crossings that do not meet current Americans with Disabilities Act (ADA) standards.

The goal of the project is to address the roadway deficiencies while enhancing the multi-modal transportation accommodations and safety.

Local Program Project

- Partially federally funded
- WisDOT Facilities Development Manual (FDM) guidelines
- Federal Environmental Review Process
 - Includes Section 106, Section 4(f), Environmental Document, etc.
- Americans with Disabilities Act (ADA) compliance
- Complete roadway reconstruction
 - New concrete pavement, curb and gutter, and sidewalk
 - New storm sewer, sanitary sewer, and watermain
 - New street lighting



The project is partially funded with federal "STP-Urban" funds. The project will need to follow Wisconsin Department of Transportation (WisDOT) guidelines and the federal Environmental Review and Documentation process. ADA standards will be followed for the sidewalks and pedestrian crossings.

The scope of the project will be a full street reconstruction, including new pavement, curb and gutter, and sidewalk. The underground utility facilities, as well as the street lighting, will also be included while the street is being reconstructed.



Proposed Improvements

This next portion of the presentation describes the proposed improvements in more detail.

Existing Typical Cross-Section



To give an idea of how the proposed work compares with the existing conditions, here is a display showing the existing cross-section dimensions for the majority of the project length. The total curb to curb width is currently 40 feet, generally, with parking on each side of the street. There are currently no bicycle accommodations in the street cross-section.

Proposed Typical Cross-Section

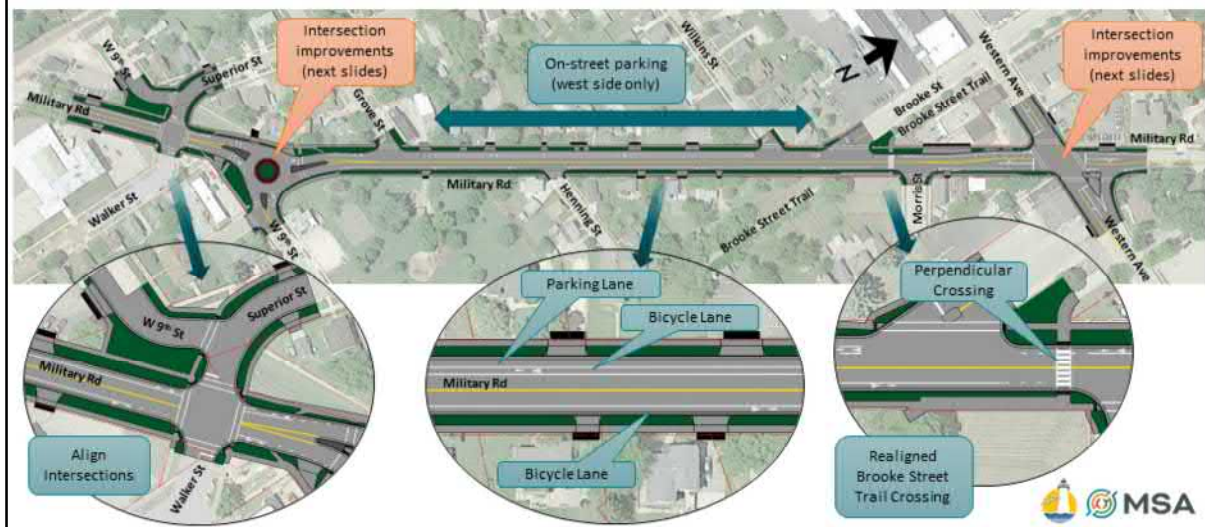


5'	7'	8'	5'	11'	11'	5'	7'	5'
SIDEWALK	TERRACE	PARKING	BIKE	TRAVEL LANE	TRAVEL LANE	BIKE	TERRACE	SIDEWALK



Here is a display showing the proposed cross-section dimensions for the majority of the project length. The total curb to curb width will be 41 feet, generally, with parking on the west side of the street only. There will be bicycle width accommodation between the curbs, as shown here. The sidewalks will generally be 5 feet wide compared to 4.5 feet wide existing.

Corridor Overview



This exhibit shows the proposed roadway overlaid onto an aerial photograph of the area. On the left, south end, the Superior Street intersection will be adjusted so it comes into Military Road at a 90 degree angle and will line up with Walker Street, for safety. Note on the exhibit that the west-side, on-street parking will extend between Grove Street and Brooke Street. At Brooke Street, the multi-use trail will be re-aligned so it crosses 90 degrees to the street, for safety.



Intersection Evaluation

West 9th Street and Western Avenue Intersections

Next we will discuss alternatives for the eastern leg of the West 9th Street intersection and the Western Avenue intersection.

Intersection Control Evaluation

- Intersections evaluated
 - West 9th Street intersection
 - Western Avenue intersection
- Evaluation factors
 - Safety
 - Operational analysis
 - Construction cost
 - Right of way impacts
 - Pedestrians and bicycles
- Improvements considered
 - Improved traffic signals
 - Roundabouts



Each of the two currently signalized intersections on the project were evaluated for safety, operations, cost, right of way impacts and pedestrians and bicycles. Improvements considered were improved traffic signals and roundabouts.

West 9th Street – Intersection Evaluation

- No significant existing safety or operational concerns observed
- Given cost and scope of Military Road reconstruction, intersection alternatives are being explored
- Existing signalized intersection has exclusive turn lanes on each approach



At the eastern leg of West 9th Street, no existing safety or operational concerns were observed. Alternatives were evaluated to incorporate the best solution for this site given the scope of the entire project. It should be noted the existing intersection has exclusive turn lanes on each approach.

West 9th Street – Improved Traffic Signal Alternative



- Removes northbound right turn lane onto 9th Street
 - Not needed for operations
- Removes small median on 9th Street
- Incorporates bicycle lanes
- Fits within existing right of way
- Crashes expected to be similar to existing intersection



The “Improved Traffic Signal” alternative for West 9th Street removes the northbound right turn lane that is not required for operations, removes the small raised median on the east leg, and incorporates the bicycle lanes. This alternative generally fits within the right of way, with some temporary construction easements required for slope and driveway blending. Crash rates would be anticipated to be similar to existing conditions.

West 9th Street – Roundabout Alternative

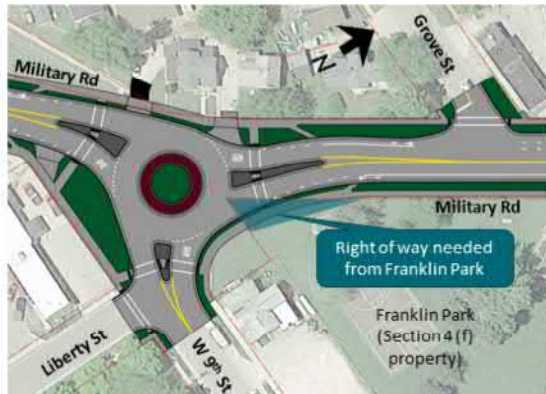


- Single lane approaches
 - Reduces number of conflict points
 - Reduces crossing distance for pedestrians and bicyclists
- Provides bicycle ramps
- Physically slows drivers
- Provides operational benefits during all hours of the day
- Requires right of way from Franklin Park (Section 4(f) property)



The “Roundabout” alternative for West 9th Street would have single lane approaches, reducing the number of crash conflict points, reducing the crossing distance for pedestrians and bicyclists, and providing bicycle ramps on each leg. A roundabout will physically require drivers to slow down in the area. Operationally, it provides for smooth and steady traffic flow during all hours of the day. This alternative would require a transference of a small wedge of land from Franklin Park to street right of way.

West 9th Street – Preferred Alternative



- Roundabout
- Encourages slow speeds through the corridor
- Has enhanced pedestrian and bicycle accommodations
 - Shorter crossings
 - Off-road bicycle path
- Less lanes and conflict points
- Provides operational benefits during all hours of the day



The preferred alternative for West 9th Street is the “Roundabout” alternative. It encourages slower speeds through the corridor, enhances pedestrian and bicycle accommodations, has less lanes and crash conflict points, and provides operational benefits during all hours of the day.

Western Avenue – Intersection Evaluation

- No existing operational concerns observed
- Angle crashes most prevalent
- Turn lanes are not aligned, reduces visibility



At the Western Avenue intersection, no traffic operational concerns were observed. When the crash history was evaluated, it was found that most prevalent were angle crashes. The existing turn lanes are not aligned with each other, which reduces visibility of oncoming traffic.

Western Avenue – Improved Traffic Signal Alternative

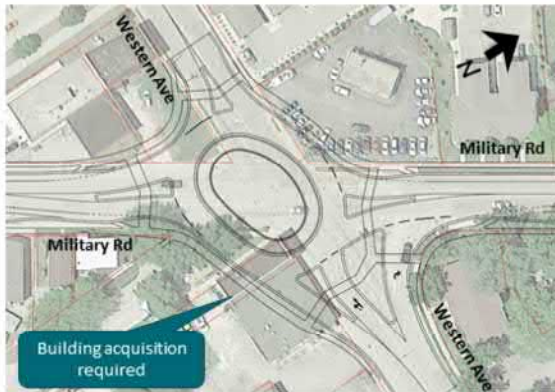


- Maintains same lane configuration
- Realigns turns lanes opposite each other
 - Improves visibility
- Incorporates bicycle lanes
- Requires right of way in corners for curb ramp improvements
- Crash types expected to be similar to existing intersection



The “Improved Traffic Signal” alternative for the Western Avenue intersection maintains the same lane configuration but realigns the turn lanes to line up opposite of each other. This would improve the visibility of oncoming traffic prior to making the turn. This alternative would require some small additional right of way in the corners at the sidewalk curb ramps. Crashes would be anticipated to be similar to existing conditions, with possibly some reduction in angle crashes due to the improved turn lane alignment.

Western Avenue – Roundabout Alternative



- Single lane approaches
 - Reduces number of conflict points
 - Reduces crossing distance for pedestrians and bicyclists
- Provides bicycle ramps
- Physically slows drivers
- Crashes expected to be less severe, but property damage only crashes may increase
- Requires right of way from several properties and one building acquisition



The “Roundabout” alternative for Western Avenue would also be single lane, have reduced crash conflict points and would provide for pedestrian and bicycles. It would physically require drivers to slow down, and crashes would be less severe. This alternative has a very large impact to adjacent properties, including a full building acquisition.

Western Avenue – Preferred Alternative



- Improved Traffic Signal
- Less costly and less impacts than roundabout
- Improved safety and visibility with realigned turn lanes
- Has enhanced pedestrian and bicyclist accommodation compared to existing intersection



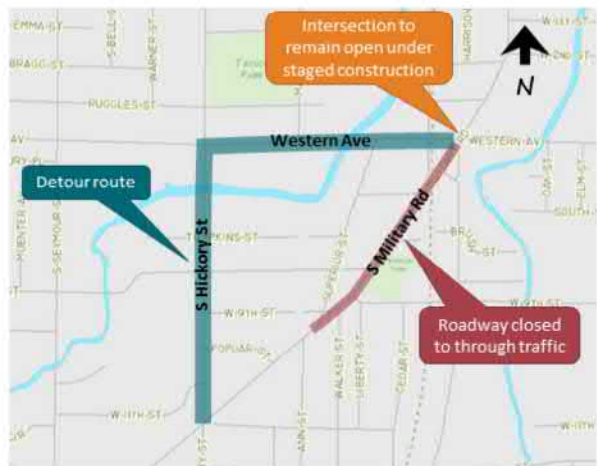
The preferred alternative for Western Avenue is the “Improved Traffic Signal” alternative. It is less costly and has much less impacts to adjacent properties. It will have improved safety and visibility with realigned turn lanes and the pedestrian and bicycle accommodations will be improved to current design standards.



Traffic & Real Estate Impacts

Next, we will discuss the traffic and real estate impacts for the project.

Construction Staging and Detour



- Military Road to be closed within project limits
- Local access provided to commercial and residential properties
- Detour using S Hickory Street to Western Avenue
- Military Road and Western Avenue intersection to remain open (staged construction) under all-way stop control
- Construction planned for spring 2022 through fall 2022



During the 2022 construction season from spring through the fall, Military Road will be closed to through traffic between the south side of Superior Street and Western Avenue. The contractor will be required to maintain at least a gravel access to adjacent properties throughout the duration, either from one side or the other.

The City will detour through traffic using Hickory Street from the south, then easterly along Western Avenue, back to Military Road. The Western Avenue intersection will be constructed in stages in order to keep traffic open to the east, west, and north using temporary stop sign control.

Real Estate

- Temporary construction easements anticipated throughout corridor
 - Minor slope-blending behind sidewalks and driveways
- Additional right of way for sidewalk and intersection improvements
- City expects to begin acquisition process winter 2020 through 2021



The project will require small strips of Temporary Construction Easements throughout the project length. There will also be some small areas of additional permanent right of way for the sidewalks, curb ramps, and intersection improvements. The City expects the real estate acquisition process to begin in the winter of 2020-2021.

Areas of Interest

Section 4(f) Resources



This slide shows the effects of the projects to the aforementioned Areas of Interest. At the Franklin School and Brooke Street Trail, a temporary construction easement will be required for blending slopes behind the sidewalk. At Franklin Park, a small area of additional right of way will be needed from the park for the construction of the roundabout and adjacent sidewalk.



Next Steps

Next, we will discuss the next steps in the project development process.

Project Schedule

- Preliminary Design Plans completed – summer 2020
- Environmental Document completed – summer 2020
- Public Involvement Event #2 – fall-winter 2020
- Real estate acquisition process begins – winter 2020-2021
- Final Plans completed – November 1, 2021
- Let Date – February 8, 2022
- Construction – spring 2022 – fall 2022



After the preliminary plans and environmental document are completed in the summer of 2020, a 2nd Public Involvement Event will be held in conjunction with the beginning of the real estate acquisition process. The 2nd Public Involvement Event, hopefully held face to face at a location in the City, will be further along in the design process, and more detail will be able to be displayed on specific property impacts.

The final plans are due in the fall of 2021. Construction is planned for the full 2022 construction season, from spring to the fall.

Public Input and Comments

- Comments on the proposed design are encouraged
- Help to develop a project that serves the needs of the community
- Email or mail comments by June 5, 2020
- For more information contact:

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This concludes the Public Involvement Event for this project. Comments on the proposed design are encouraged. Please email, mail, or phone your comments to Quirin Klink of MSA or Paul De Vries of the City of Fond du Lac by June 5, 2020. Thank you for taking the time to watch this presentation, and have a great day.